



# Welcome!

## United Counties of Stormont, Dundas and Glengarry County Road 34 (Main Street) and Mill Square Improvements Public Information Centre

Welcome to the online Public Information Centre (PIC) for the County Road 34 (Main Street) and Mill Square Improvements Environmental Assessment Study.

At the present time, the Province of Ontario has implemented a State of Emergency requiring measures to deal with the COVID-19 pandemic and has prohibited public gatherings. As a result, this PIC is relying on web-based communications. Should you have any questions regarding the study, please contact the United Counties' or Consultant Project Managers listed below.

Several background reports are available to supplement the information shown in the exhibits. Should you have any questions regarding the materials or any other aspect of the study, please contact the following by **August 5, 2020**.

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There is an opportunity at any time during the Class EA process for interested persons to provide written input. Any comments received will be collected under the Environmental Assessment Act and, with the exception of personal information, will become part of the public record.

# Introduction

The United Counties of Stormont, Dundas and Glengarry is conducting this Municipal Class Environmental Assessment (EA) for improvements to: **County Road 34 (Main Street) from County Road 10 (Lochiel Street) northerly to Elgin Street; and Mill Square in downtown Alexandria.** The Study has developed and evaluated alternatives for the roadway cross section, intersections, active transportation features, and drainage, and has determined the property requirements to implement the project.

This Study was initiated as a Schedule A+ EA Study, based on the range of anticipated effects. Based on the recommendation, the study has been elevated to a Schedule B EA Study and will meet the requirements of the Municipal Class EA process.

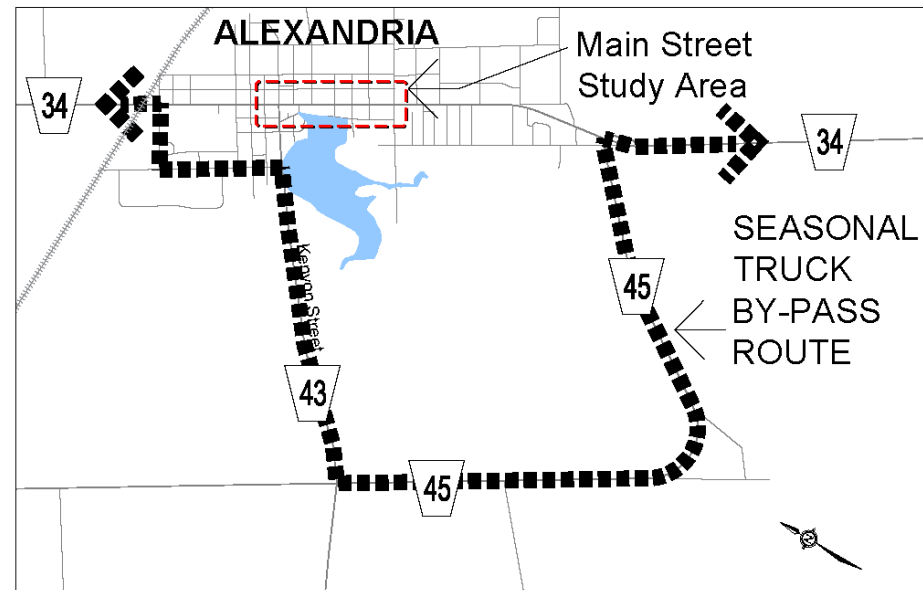
A Final Study Design Report describing the study process has been made available for agency and public comments. It is available on the United Counties website:

<https://sdgcounties.ca/node/1090>



First paving of Main Street (1910)

# Study Area



# Municipal Class Environmental Assessment (Class EA) Process

**This project is being completed as a Schedule B Class EA based on the range of anticipated effects in accordance with the Municipal Class Environmental Assessment, (2015).**

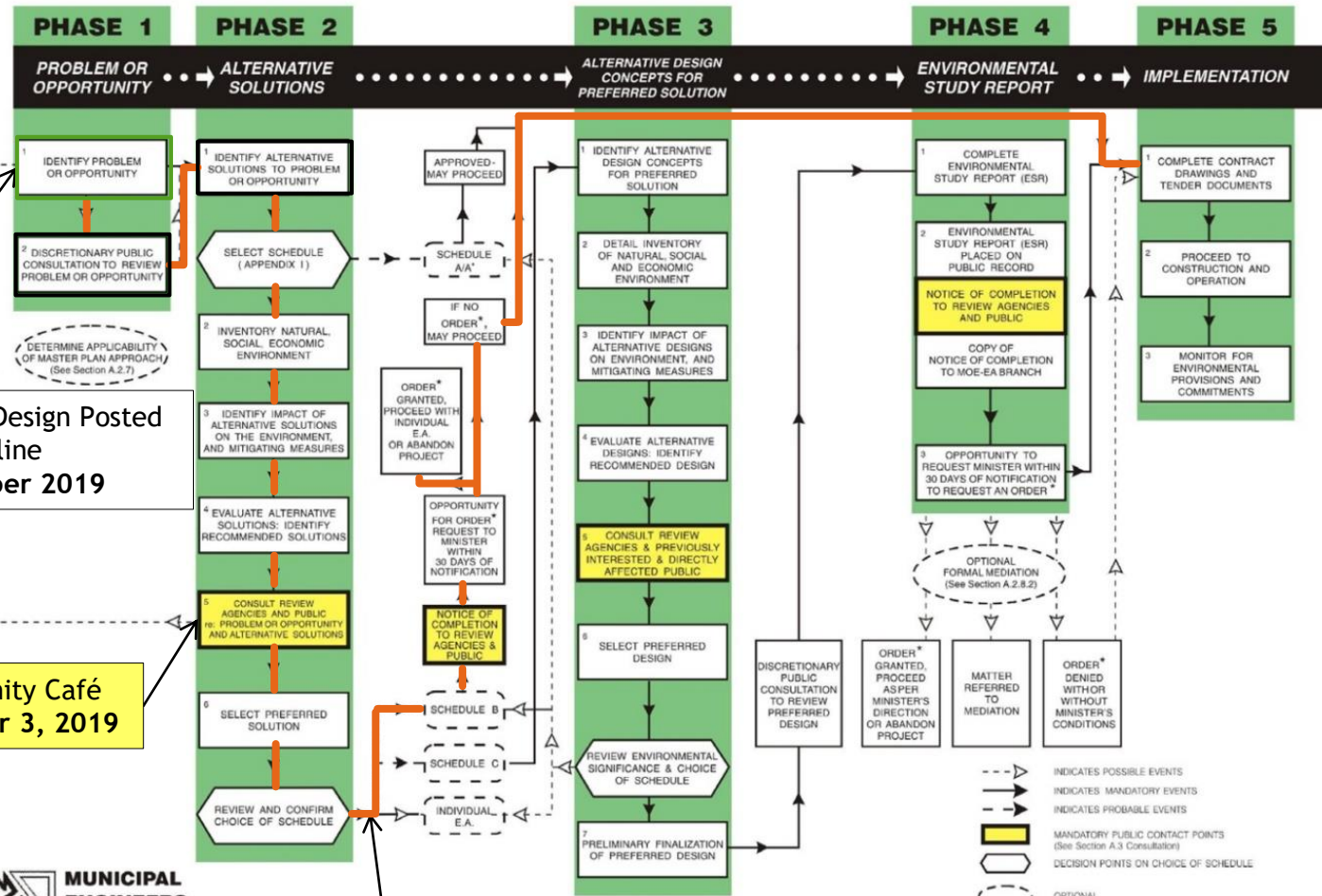
The EA Study will be documented in a Project File, which is a summary of all public consultation, data, recommendations and reports produced for the project.

If after viewing the PIC exhibits and making your concerns known to the project team, you still have concerns at the end of the process, you have the right to request the Minister of the Environment, Conservation and Parks to reclassify the project through a Part II order (or “bump-up”) to an Individual Environmental Assessment.

The Municipal Class EA Process Flowchart is illustrated on the following exhibit. Schedule B projects completed Phases 1 and 2 prior to proceeding to Phase 5. A discretionary public consultation event (this online Public Information Centre) has been added to the Municipal Class EA Process.

# Municipal Class Environmental Assessment (Class EA) Process

**NOTE:** This flow chart is to be read in conjunction with Part A of the Municipal Class EA



Draft Study Design Posted  
Online  
November 2019

Community Café  
December 3, 2019

Online PIC  
July 22, 2020 to  
August 5, 2020



Study process is here (PIC No. 1)  
Schedule B Process

# Island Park and Mill Square

The Community Improvement Plan (CIP) for North Glengarry was completed in 2015 for designated Community Improvement Project Areas to create and preserve a traditional “Main Street”, where people can stroll, relax, participate in various activities and shop in a charming and secure atmosphere that is highlighted through its architecture and physical setting. Alexandria was identified as a Community Improvement Area.

The goals and objectives of the CIP include:

- ▶ Promotion of Mill Pond through improved pedestrian access to this shoreline from Main Street and Mill Square.
- ▶ Preservation, beautification and promotion of key public spaces, such as Mill Square, and their reinforcement as the heart of their respective downtown areas.
- ▶ Harmonious cohabitation of pedestrians and vehicles, through improvements to pedestrian crosswalks, sidewalks, parking and a reduction in the nuisances caused by heavy truck traffic.



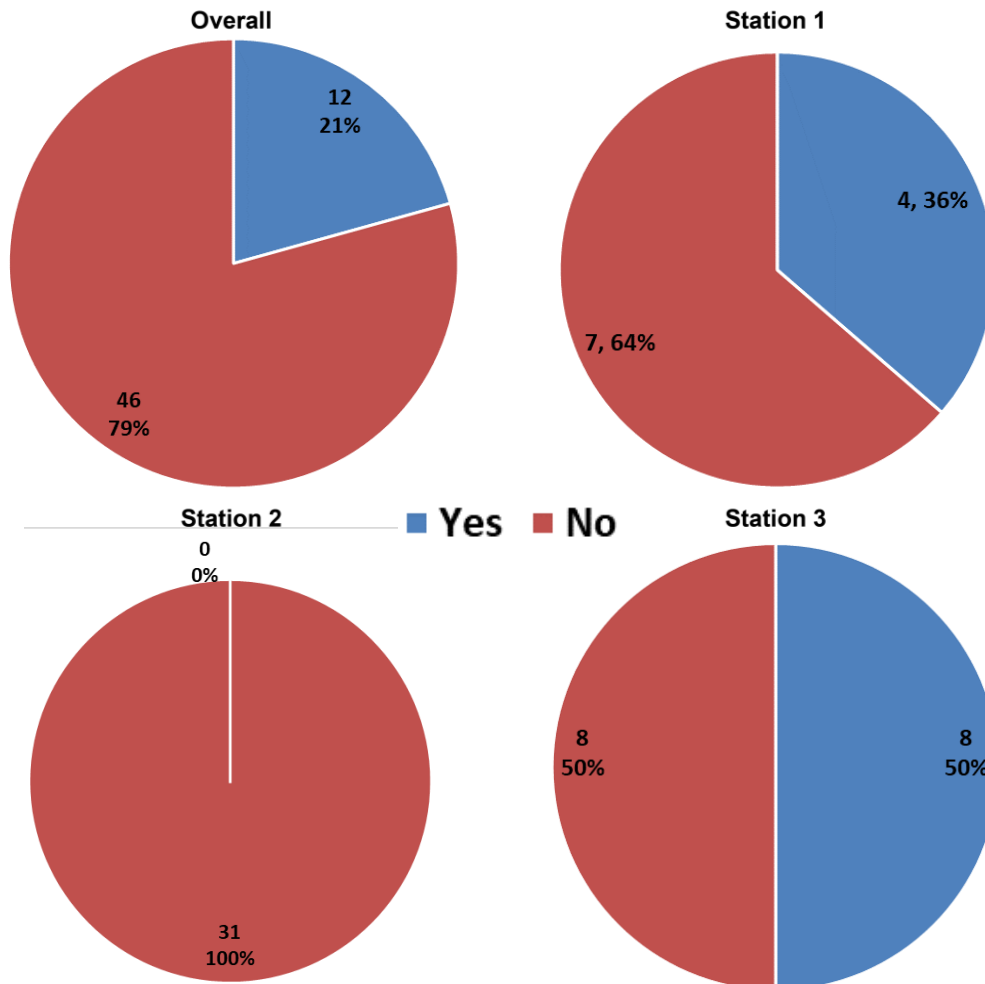
# Study Area Issues

- ▶ Close proximity of adjacent buildings
- ▶ Limited physical space for cyclists
- ▶ Through truck movements
- ▶ Lack of pedestrian crossovers
- ▶ On-street parking review
- ▶ Alexandria United Church Cemetery
- ▶ Opportunity for Complete Street
- ▶ Snow storage



# Truck Bypass Utilization

An Origin/Destination (O/D) survey was completed in the fall of 2019 for vehicles entering and/or exiting Alexandria. Heavy vehicle traffic through the town was evaluated based on this survey.

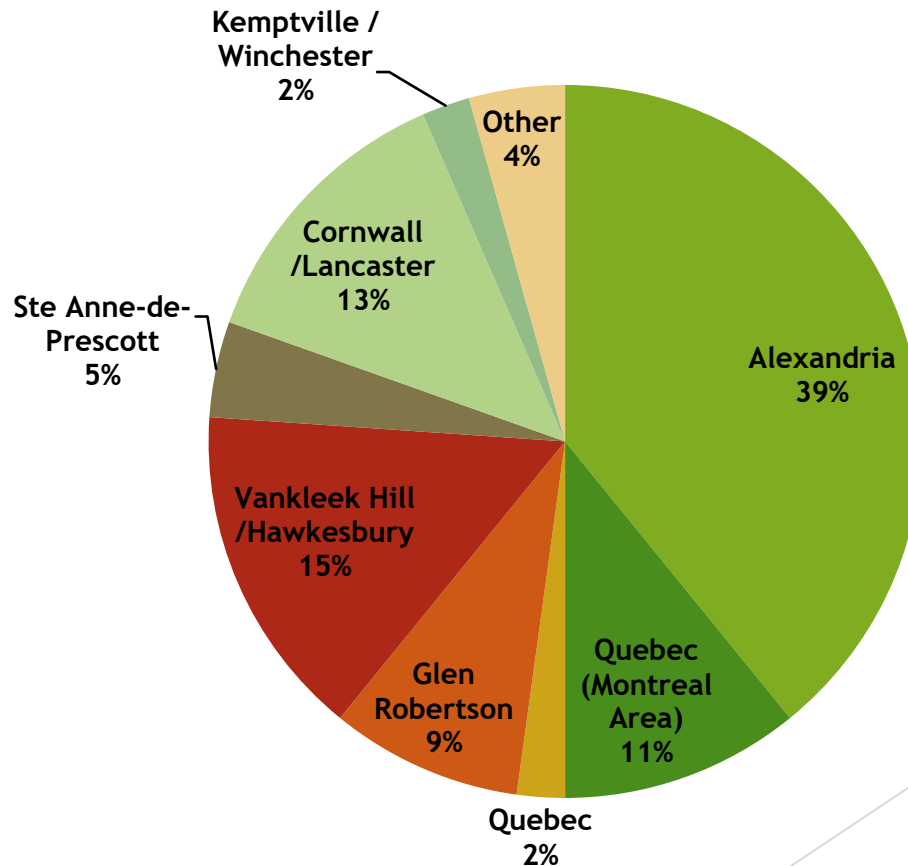


The origin destination survey confirmed that the existing truck bypass routes carry a limited percentage of the existing truck traffic.

Station 2 was located beyond the intersection with County Road 45, so none of the traffic surveyed had taken the bypass. The utilization of County Road 45 as a bypass route is reflected in the volume of truck traffic.

# Truck Bypass Utilization

The destinations of heavy trucks surveyed during the O/D Survey are illustrated below. Approximately 2/3 of trucks surveyed have a destination outside of Alexandria and could make use of a truck bypass.



# Community Café

The first Community Café was held on December 3, 2019 in Alexandra. Four topics were discussed with the 18 registered attendees: Safety (Pedestrians and Cyclists); Traffic Operations; Businesses; and Design Approach.

Comments included:

- ▶ Improve the truck bypass to remove trucks from Main Street;
- ▶ Better signage (truck bypass and parking);
- ▶ Traffic calming on Main Street;
- ▶ No right on red at County Road 34/County Road 43;
- ▶ Vehicular through traffic is good for business;
- ▶ Consider burying utilities; and
- ▶ Provide additional parking lots in combination with removing on-street parking.

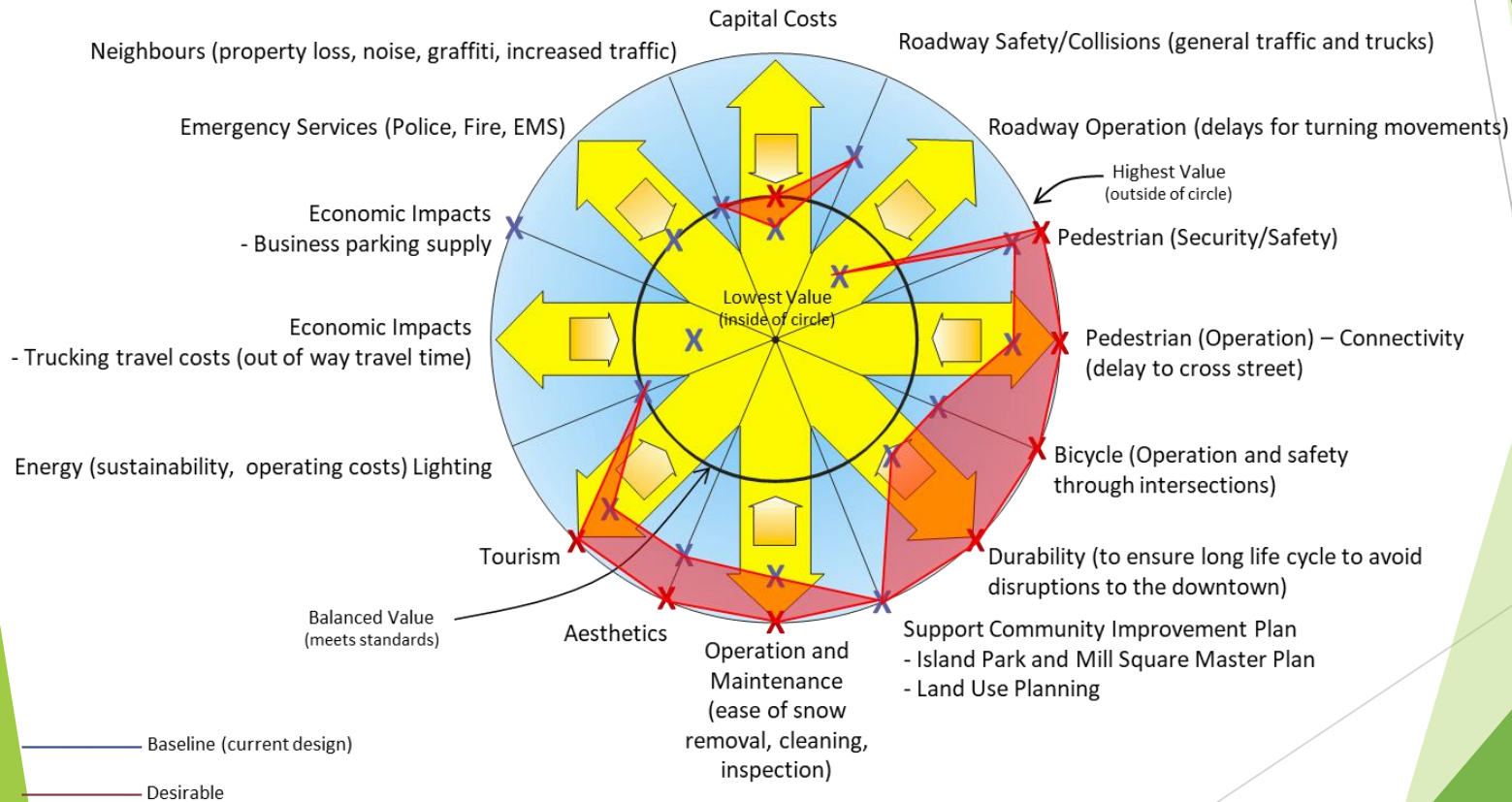


# Value Planning Workshop

- ▶ Held November 20, 2019 at the Sandfield Centre in Alexandria with representatives from the United Counties, the Township of North Glengarry, Raisin River Conservation Authority and the Eastern Ontario Health Unit.
- ▶ Quality Model defined expectations for “performance” to be delivered by the project
- ▶ A FAST diagram was developed to better understand the objectives to be solved as part of the study, including creating a downtown by beautifying downtown Alexandria
- ▶ A list of creative ideas was developed at the workshop

# Quality Model

- The Quality Model was created at the Value Planning workshop by the Project Team to define expectations for “performance” to be delivered by the project. The X’s on the outside of the circle define the desired areas to provide the most performance.



# Built and Cultural Heritage

- ▶ A Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) checklist was completed (Built and Cultural Heritage checklist) for the Study Area.
- ▶ The Study Area contains buildings over the age of 40 years old that may be impacted by the proposed works.
- ▶ One or two properties are recommended for further investigation due to the existing structural deterioration of the buildings. It is recommended that a Cultural Heritage Evaluation Report be completed prior to any potential use of the properties to assess the potential for heritage resources.
- ▶ Repurposing of these buildings was identified for future consideration.



Condition of Squared Timbers  
under Pigeon Building



Buried Structure



# Preliminary Design Alternatives

- ▶ Additional alternatives that were considered and not recommended are shown in a separate attachment posted on the website.
- ▶ The final study recommendations are described as potential interim and long range solutions and are illustrated on the following exhibits.
- ▶ The plans include recommended improvements to be undertaken by the Township of North Glengarry and the United Counties of Stormont, Dundas and Glengarry.

# Recommended Plan

## Recommended Plan - Phase 1 Improvements

- ▶ Traffic calming: narrow traffic lanes with greater space for sidewalks and/or parking
- ▶ Reconstruction of Main Street with new asphalt, sidewalks and decorative lighting
- ▶ New street furniture, plantings and streetscaping
- ▶ Bury utilities in the core blocks from CR43 to Gernish Street East and Mill Square (for discussion with utility companies)
- ▶ Remove commercial building (Pigeon Building) and create walkway to parking lot (future consideration)
- ▶ Provide parking and driveway link from Main Street to Ottawa Street
- ▶ No right on red and pedestrian push buttons at CR43/CR34 (being implemented)
- ▶ Parking pockets along CR34 where space is available
- ▶ Pedestrian connections to the park via Main Street, Gernish Street East and Derby Street East
- ▶ Consideration of a new parking lot/greenspace at Gernish Street East/Main Street
- ▶ Sidewalk connection from Main Street to Park Avenue on Gernish Street East and Derby Street East
- ▶ New/improved parking lot directional signage
- ▶ Improved truck route bypass signage to CR46/CR45
- ▶ Share the Road signage for cyclists on Main Street
- ▶ On-street parking on east side of road

# Recommended Plan

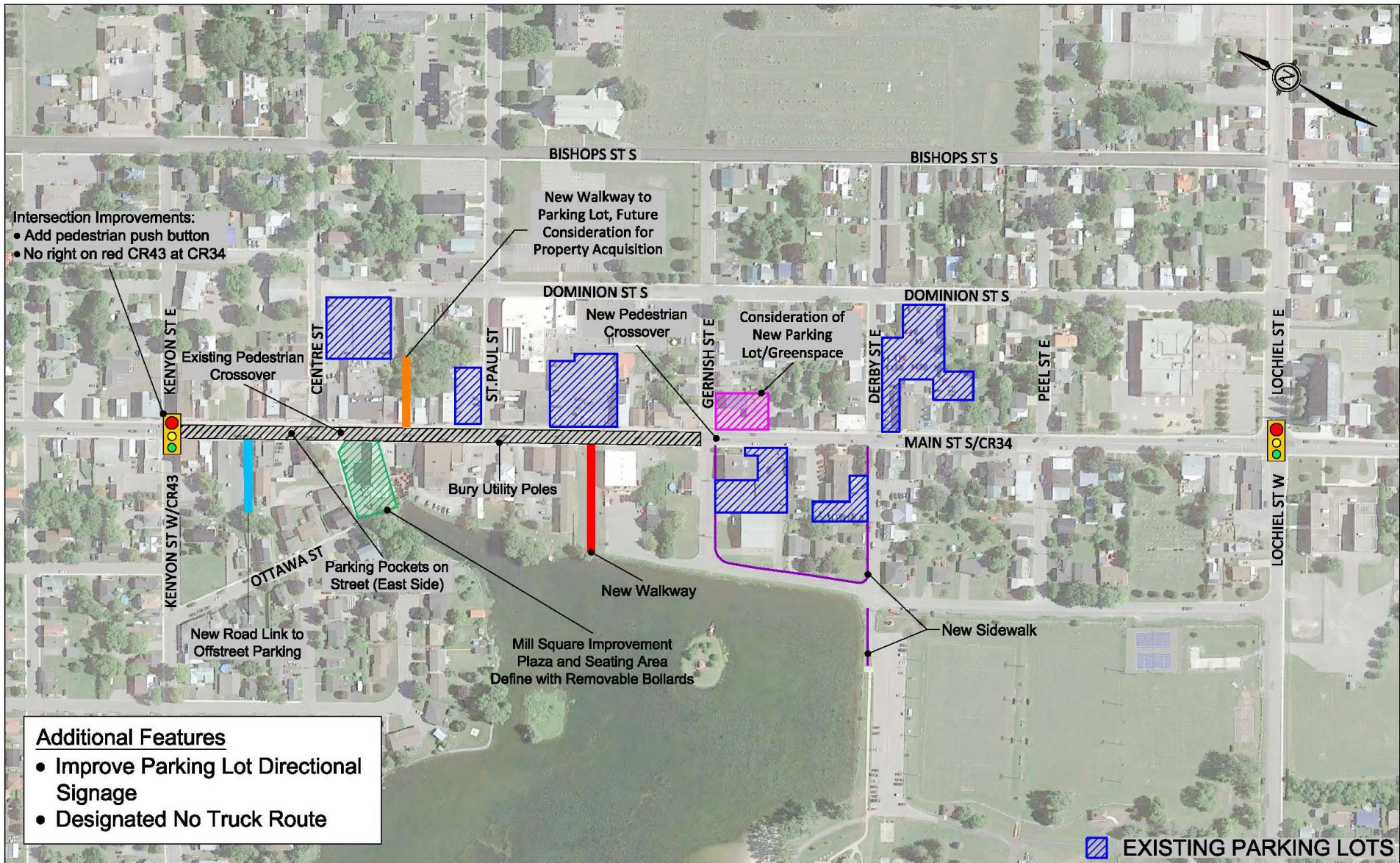
## **Interim Plan - Phase 2 Improvements (for Future Study)**

- ▶ Interim truck route bypass improvements
- ▶ Year-round truck bypass
- ▶ Roundabout control to increase truck driver awareness to follow truck route
- ▶ Improved truck operation at CR43/CR46

## **Long Range Plan - Phase 3 Improvements (for Future Study)**

- ▶ New truck route bypass

# Recommended Plan - Phase 1 Improvements





## February 2020 REV 1 July 2020



## Parking Pocket Sample



# Recommended Plan - Phase 1 Improvements

## Desirable Minimum Cross Section

The desirable minimum cross section is illustrated on the following exhibit. In order to achieve the desirable cross section, there are locations where “sliver” widening (small property acquisitions), narrow sidewalks or the removal of on-street parking is recommended.

Locations where sliver widening (small property acquisition) is recommended include:

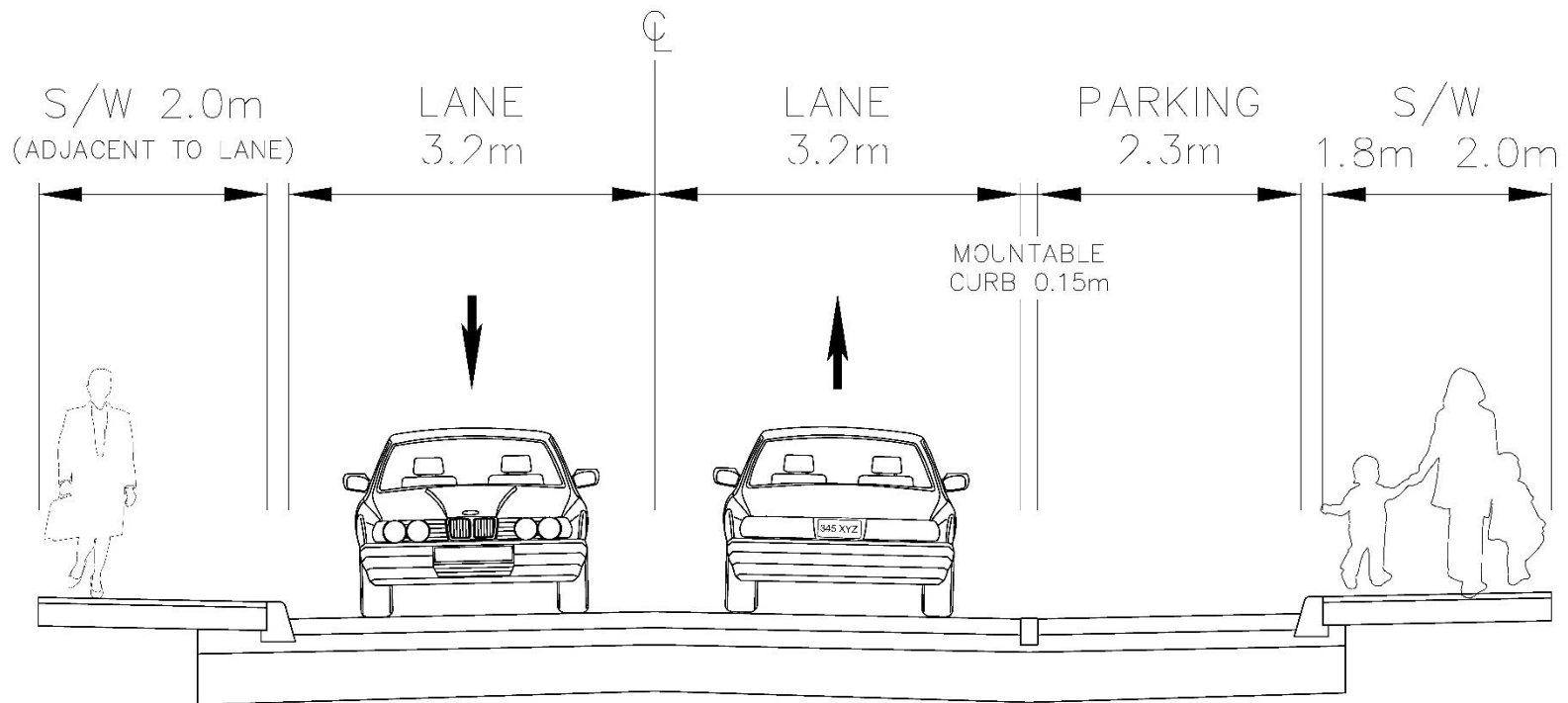
- ▶ East side between Paul Street and the Municipal Parking Lot to accommodate 2.0 m sidewalk
- ▶ West side from Municipal Parking Lot to Gernish Street
- ▶ West side from Gernish Street to Derby Street
- ▶ West side where space is available between Derby Street and Peel Street
- ▶ West side between Peel Street and cemetery

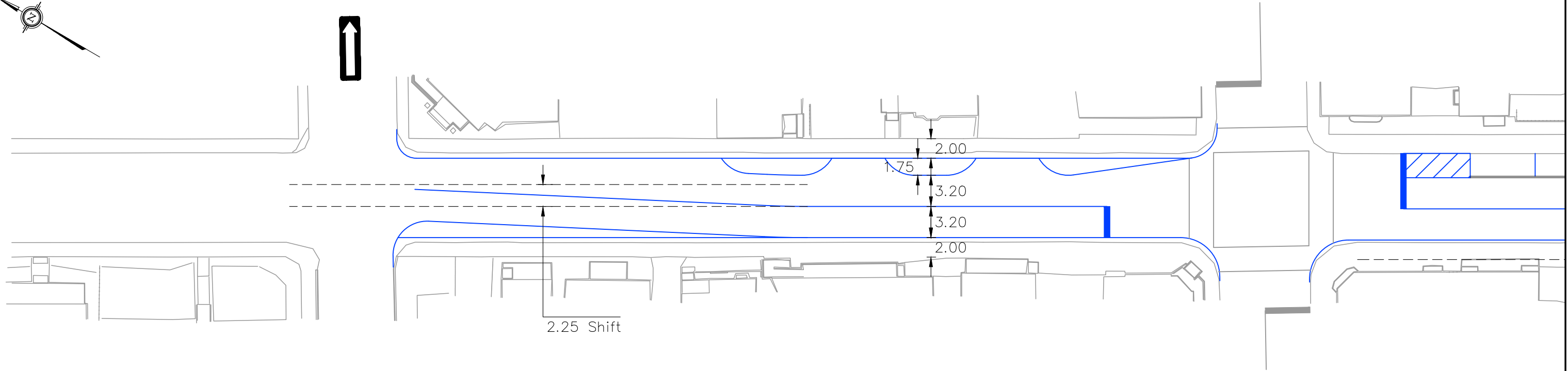
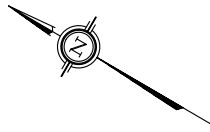
Locations where acceptance of narrow sidewalks or removal of on-street parking is recommended include:

- ▶ 1.75 m sidewalk on east side between Kenyon Street and Centre Street
- ▶ Removal of on-street parking in front of the Municipal Parking Lot
- ▶ Removal of four on-street parking spots from Derby Street southerly

# Recommended Plan - Phase 1 Improvements

## Desirable Minimum Cross Section

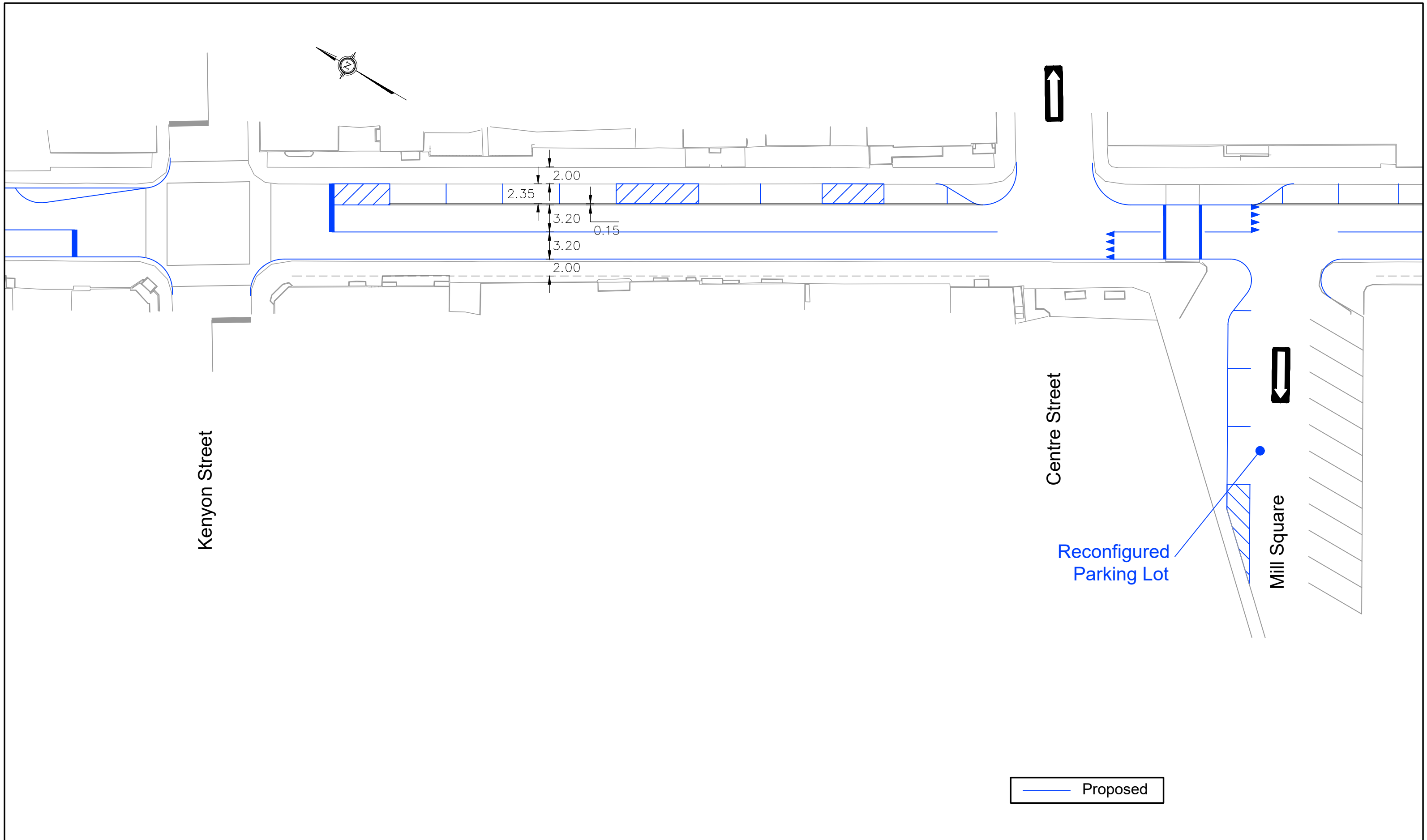


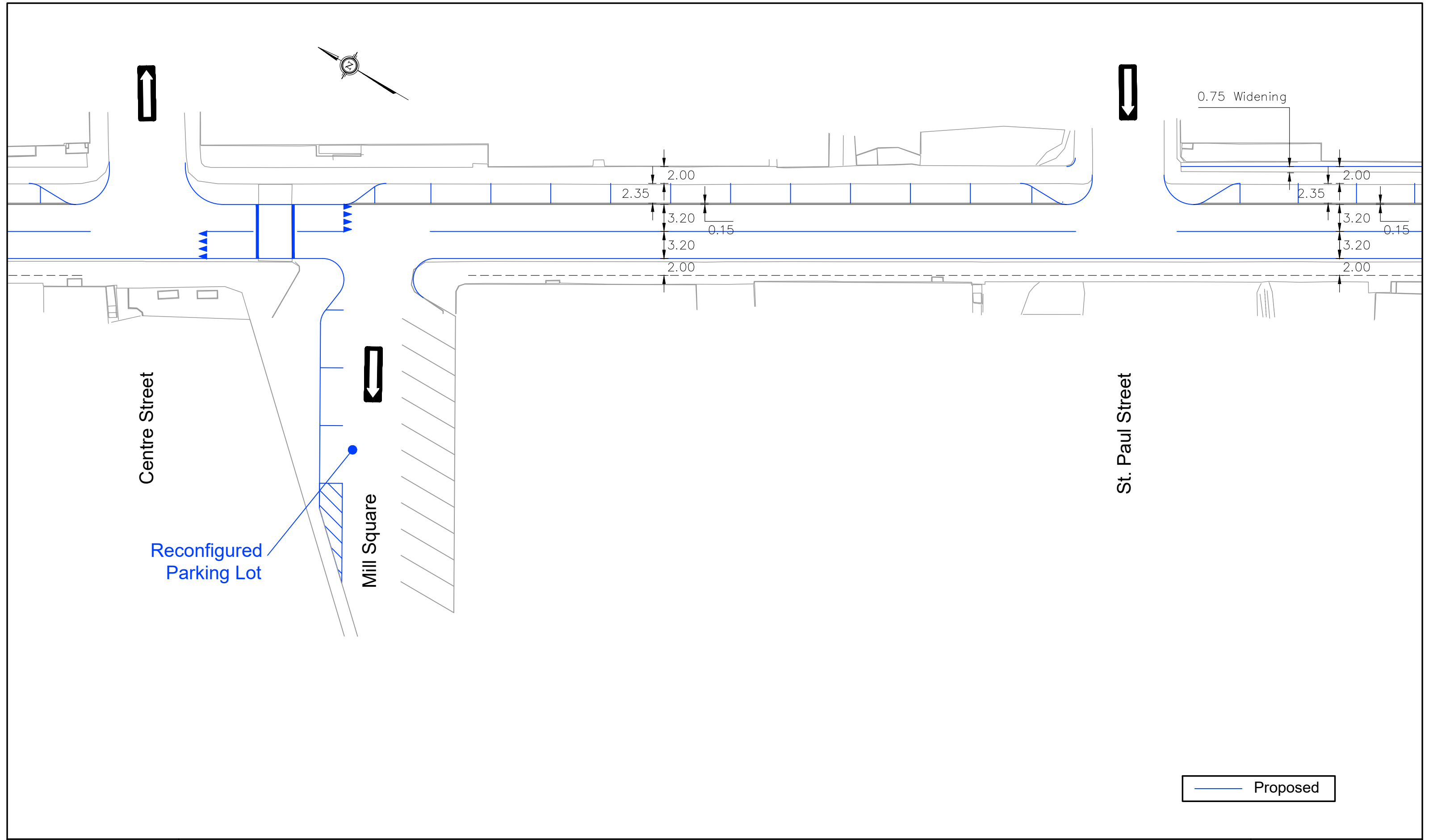


Elgin Street

Kenyon Street

Proposed

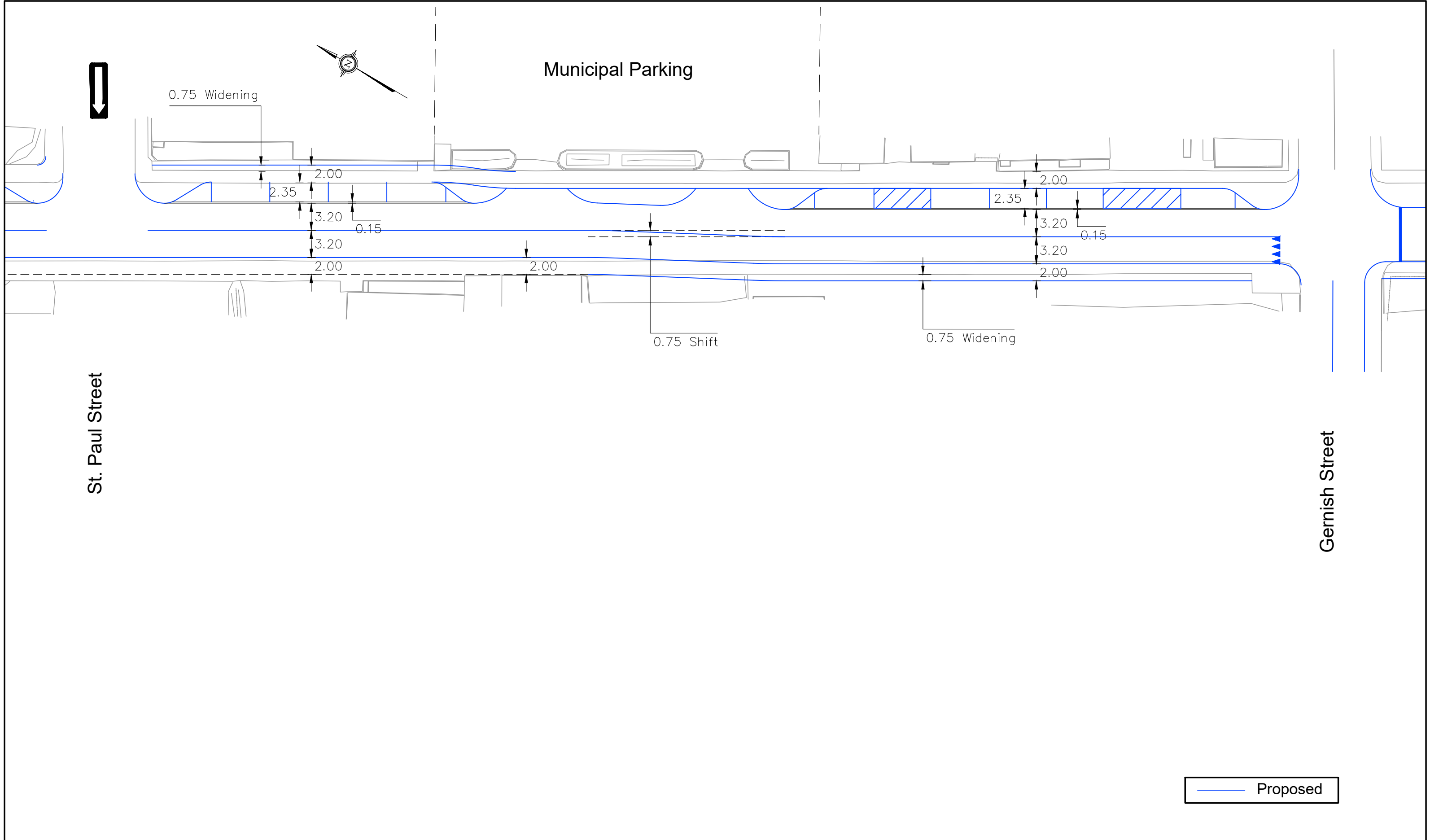




**BTE 19-034**  
2020-07-11  
Scale 1:400

**Main Street, Alexandria**  
Proposed Geometry between Centre Street and St. Paul Street

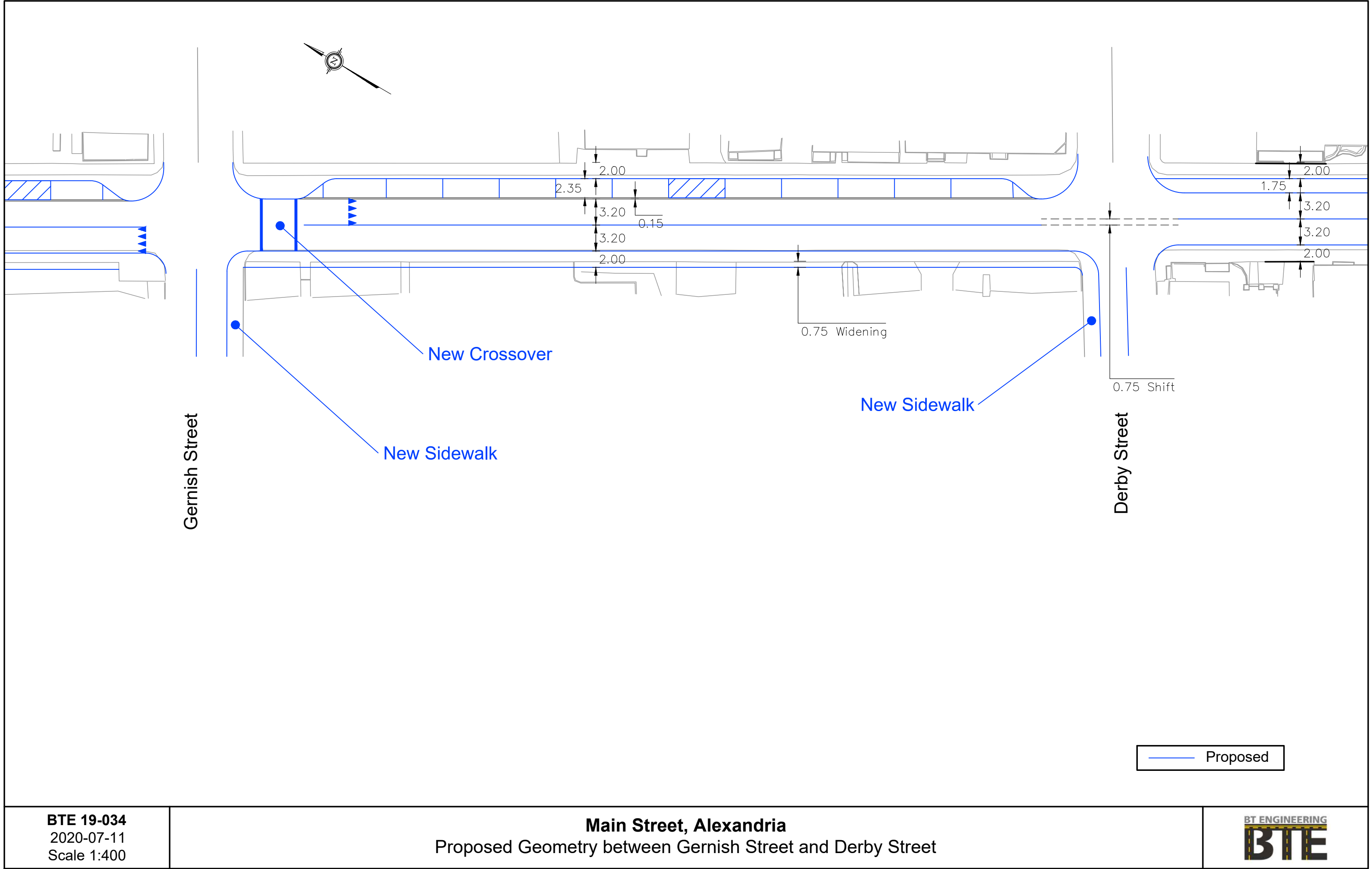


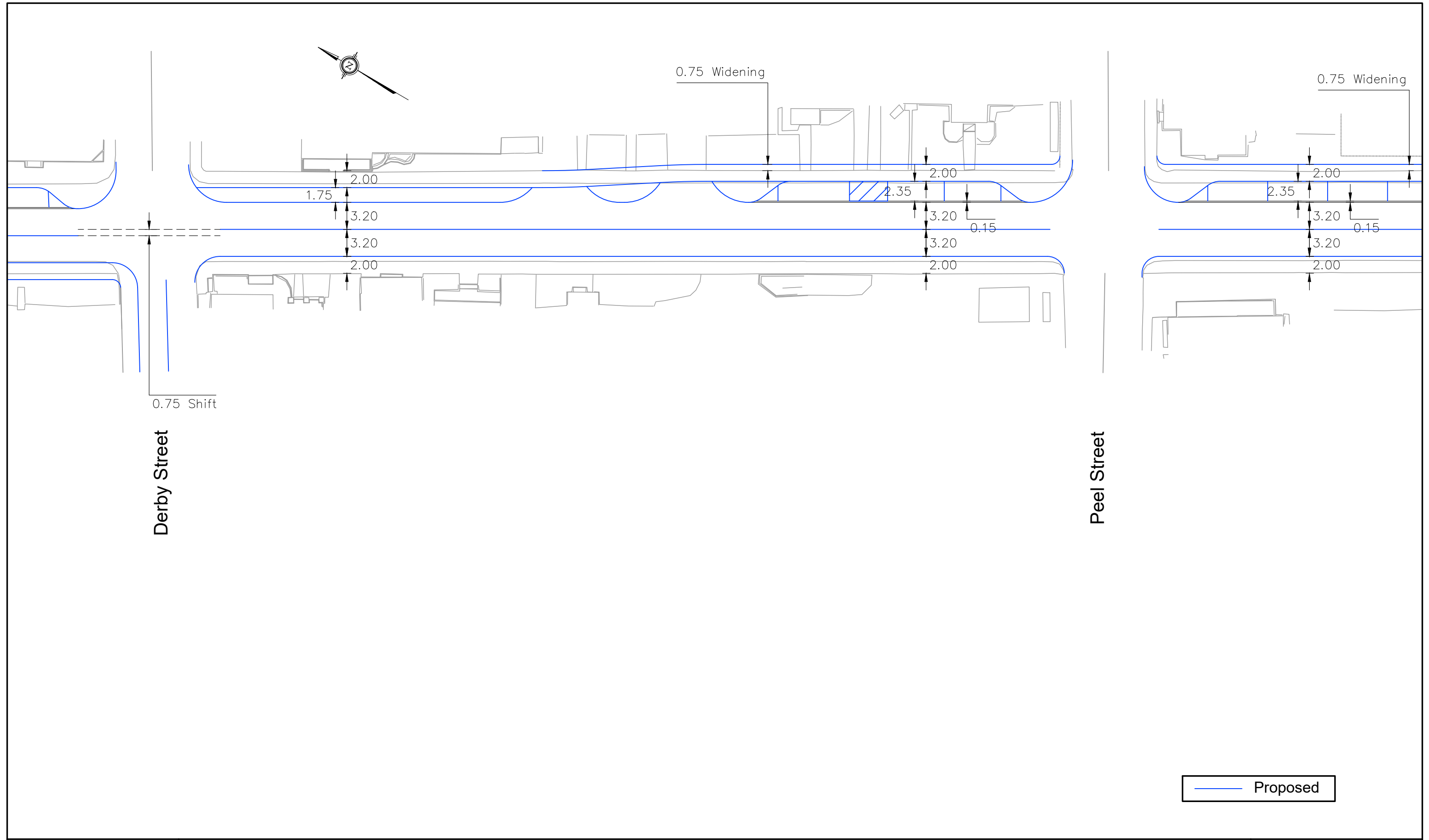


**BTE 19-034**  
2020-07-11  
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**Main Street, Alexandria**  
Proposed Geometry between St. Paul Street and Gernish Street



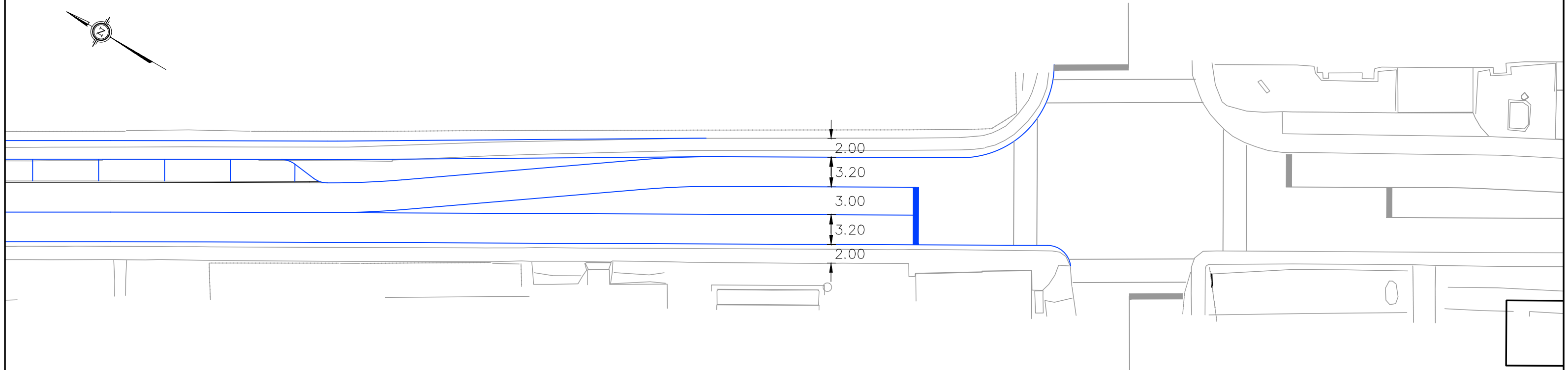
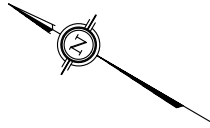




**BTE 19-034**  
2020-07-11  
Scale 1:400

**Main Street, Alexandria**  
Proposed Geometry between Derby Street and Peel Street





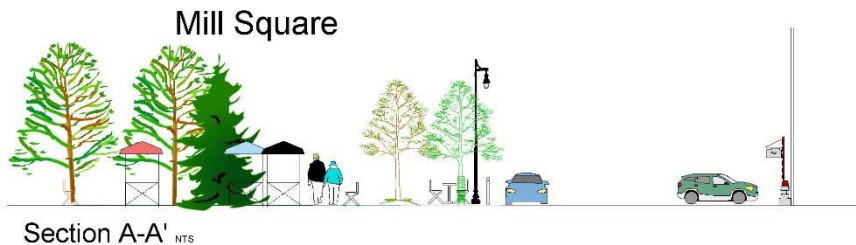
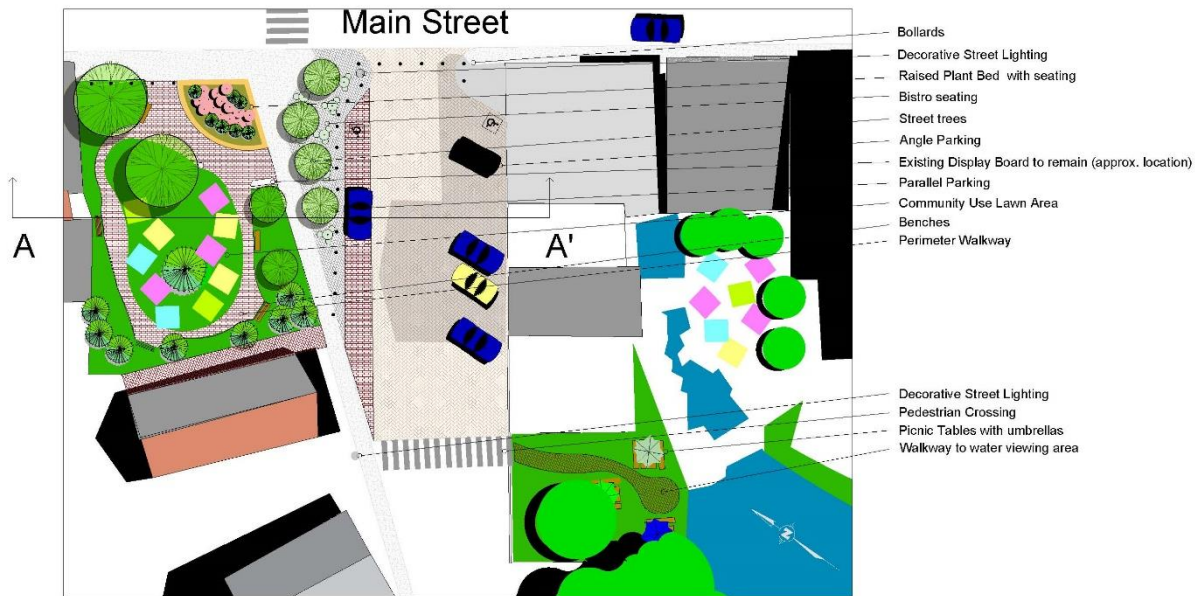
Lochiel Street

— Proposed

# Recommended Plan - Phase 1 Improvements Mill Square

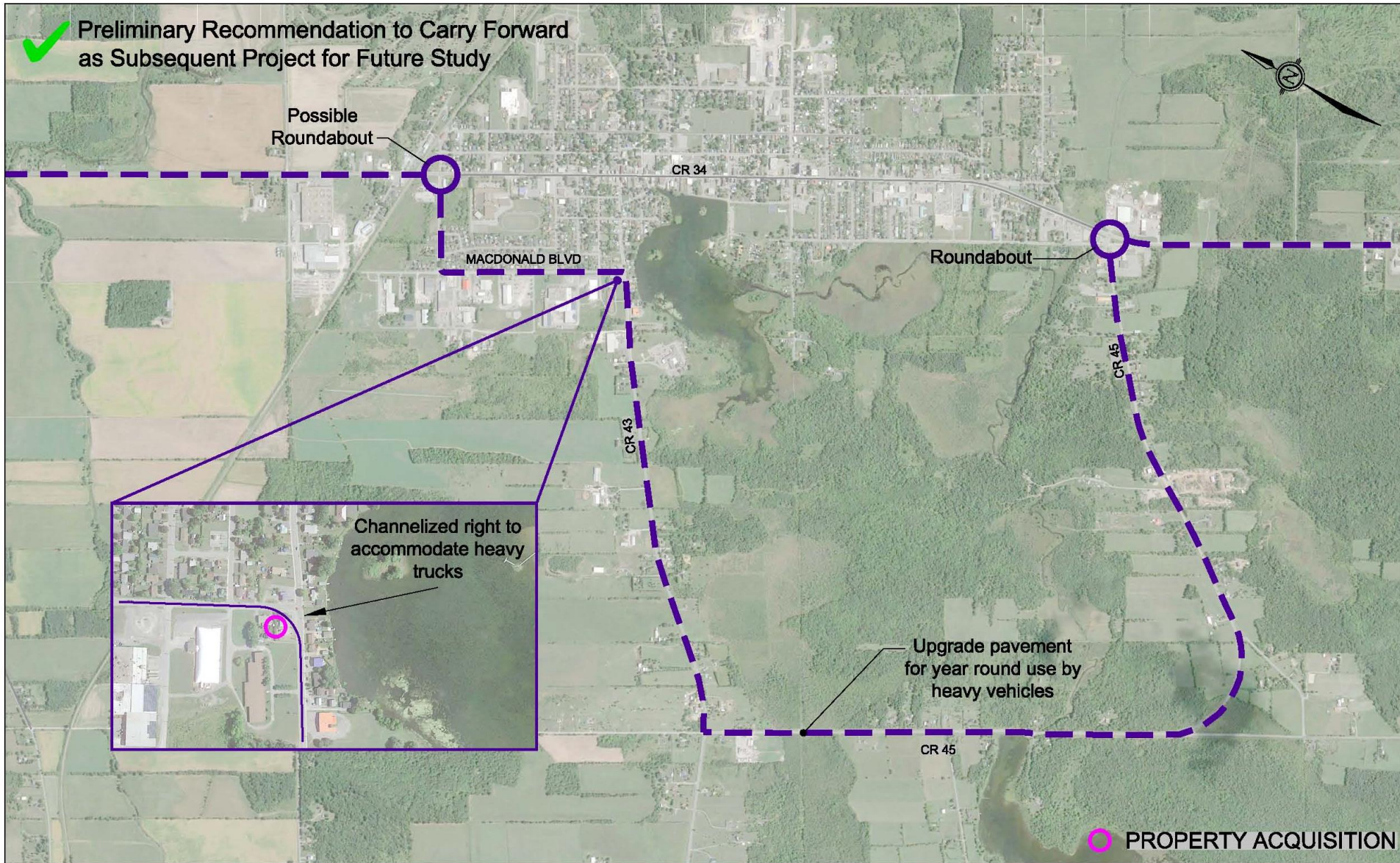
## Mill Square Phase 1 Improvements

- Replace existing light pole with decorative pole
- Bollards for decoration
- Narrow roadway and provide additional green space



# Interim Plan - Phase 2 Improvements

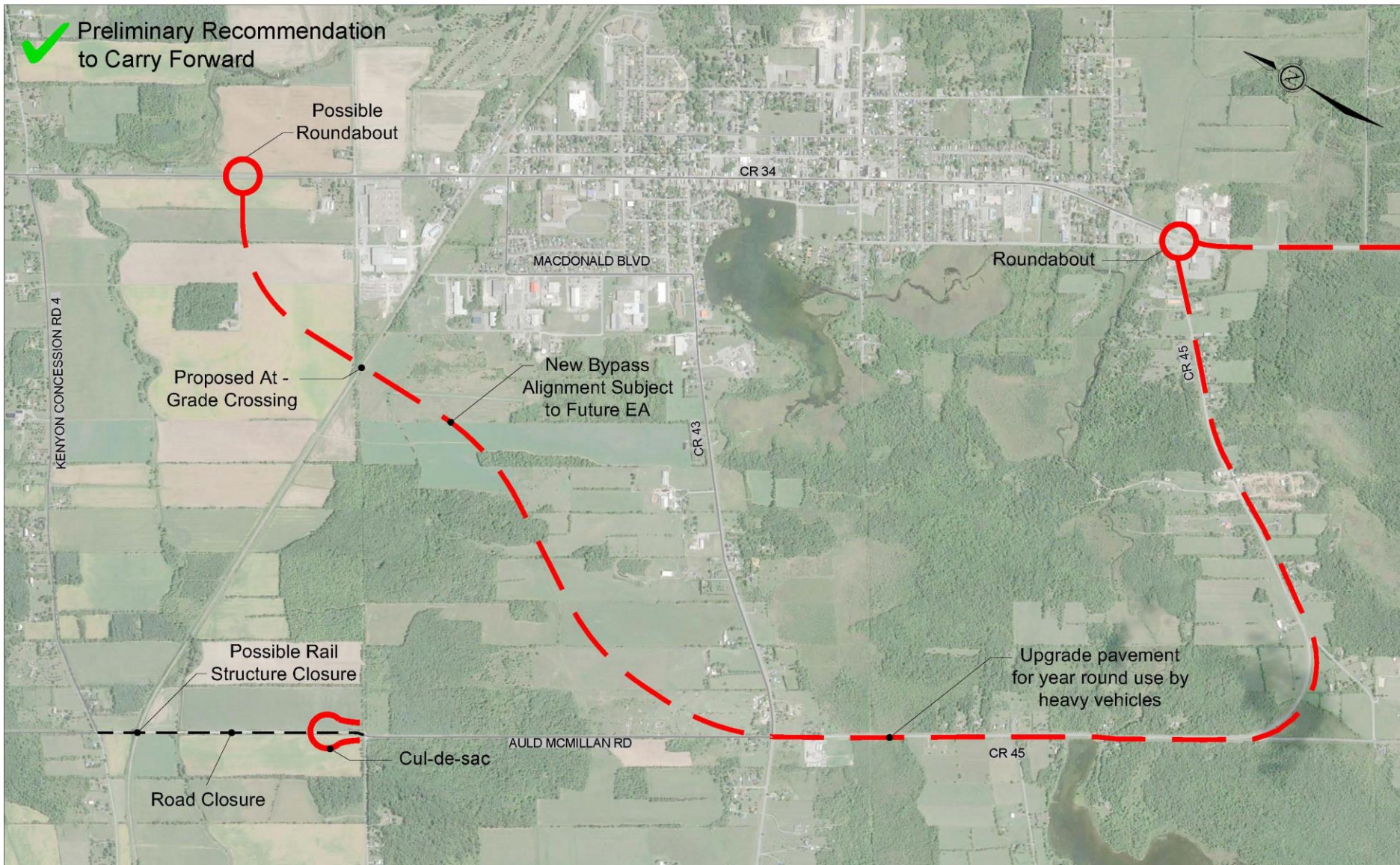
✓ Preliminary Recommendation to Carry Forward  
as Subsequent Project for Future Study



# Long Range Plan - Phase 3 (for Further Study)



Preliminary Recommendation  
to Carry Forward



# Next Steps

## Following this meeting we will:

- ▶ Review all PIC No. 1 comments and ideas and prepare a Summary Report
- ▶ Finalize the Recommended Plans
- ▶ Project File for 30-day Public Review Period

## How can you remain involved in the Study?

- ▶ Request that your name/e-mail be added to the mailing list
- ▶ Provide an online comment
- ▶ Contact the Project Managers at any time
- ▶ Contact information is shown below.

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Phone: 613-932-1515 ext. 208

## Thank you for your participation in this online PIC.

Your input into this study is valuable and appreciated.

Please submit any questions or comments to the contacts listed above by **August 5, 2020.**

All information is collected in accordance with the *Municipal Freedom of Information and Protection of Privacy Act*.